

Attach To Contract Document

New York City Department of Transportation
Division of Bridges
Office of Consultant Programs

Total Design and Construction Support Services for
Replacement of Bruckner Expressway
over Westchester Creek (Unionport Bridge),
Borough of the Bronx

Contract No. HBX1131

E-Pin No. 84111M0005

Pin No. 84111BXBR587

Addendum #1
July 29, 2011

**Please be advised NO further questions will be entertained on this
procurement**

This Addendum Is Hereby Made Part of the Contract Documents

NOTE:

Attached please find:

1. Addendum No. 1
2. Responses to Questions raised to the Agency
3. Replace Cover Page with Cover Page R
4. Replace Page 4 with Page 4R
5. Replace Page 3 of the Scope of Work with Page 3R
6. Replace Page 4 of the Scope of Work with Page 4R
7. Replace Form 4T1 with Form 4T1-R
8. Replace Form 4T2 with Form 4T2-R
9. Copy of Pre-Proposal Presentation
10. Pre-Proposal Conference Sign-In Sheets
11. Acknowledgement Receipt of Addendum #1

**Total Design and Construction Support Services for
Replacement of Bruckner Expressway
over Westchester Creek (Unionport Bridge),
Borough of the Bronx
Contract No. HBX1131
E-Pin No. 84111M0005
Pin No. 84111BXBR587**

Questions & Answers:

Q1. Are you looking to continue using the storage under the approach spans?

A1. The intended use of space under the approach span for storage purpose will be examined and evaluated during the preliminary design in consultation with the user groups at DOT. However, for the purpose of preparation of proposals, it should be assumed that the current use of space for storage shall be continued.

Q2. What is Construction Cost?

A2. The Agency does not give out the Construction Cost.

Q3. Does the total time allocated for the project include the Construction Phase?

A3. Yes. See revised Cover Page-R and see Page 2, Section B. of the RFP.

Q4. Can we receive a copy of Sign-in Sheet?

A4. Yes. See attached.

Q5. Can you make presentation slides available?

A5. Yes. See attached and note the following: This information is provided for information purpose only. Nothing in the presentation is intended to amend the requirements of the RFP document.

Q6. Is a BRPR required? There is no statement about it in Scope of Work. However the BRPR format and requirements are included in RFP and Appendix.

A6. No, a BRPR is not required. The intent is to prepare a Design Approval Document, as stated in the Scope of Work, in accordance with NYSDOT Project Development Manual (PDM), which is required for the obligation of federal funding during construction. It is not necessary to compile and submit a BRPR document as noted in the RFP and Appendix, however, it should be noted that significant information that is required for the preparation of the BRPR may also be required for the preparation of the Design Report.

Q7. Is there an MWBE goal?

A7. There is no MWBE goal in this contract. However, this contract is subject to 18% DBE goals.

Q8. What are the R.O.W. Acquisition permits for the new widened bridge?

A8. R.O.W. plans shall be prepared during the preliminary design phase to clearly establish the jurisdiction of the land required for the widened structure. All information pertaining to the R.O.W. acquisition, temporary and permanent easements, etc. shall be clearly established. A contingency allowance for ULURP has been established for this effort in the RFP.

Q9. In the RFP Page 3 under A) SUPERSTRUCTURE it reads "The existing double leaf, fixed trunnion, bascule bridge will be replaced with a new movable span of bascule type". The PowerPoint Presentation recommends a rolling lift bascule bridge. What is the intent of the Department? Is it to replace a rolling lift or other bascule types can be considered ?

A9. The information provided in the PowerPoint Presentation does not amend any requirements in the RFP. The intent of the department is for the designer to analyze various feasible bascule types and recommend the most economical and cost-effective movable span of the bascule type for consideration.

Q10. Is it the intent of the Department to perform an evaluation of the Design Alternatives? Or is it just to advance the recommended alternative per Page 2 of the RFP which contradicts item b) under 2-4 on Page 9 of the RFP?

A10. The intent of the Department is to prepare a Design Report in accordance with NYSDOT's Project Development Manual (PDM), and advance the preferred alternative to preliminary design to include all the requirements for the new bridge as per page 2 of the RFP. As noted during the presentation, a lot of engineering data has been collected, analyzed and documented during the previous contract, including the recommended alternative per page 2. It is our understanding that, the designer will validate and utilize as much information from that contract during the preparation of the Design report.

Q11. Should value engineering be performed during and after the Preliminary Design instead of or during the Final Design as indicated in subscription B (d) on Page 13 of the RFP? Please confirm if in fact the Value Engineering will be performed by NYC OMB as it has always been and that consultant will not have to include costs associated with the Value Engineering effort in their proposal.

A11. The Value Engineering workshops are usually scheduled by the Office of Management and Budget (OMB) for projects of this magnitude only when the design has advanced significantly enough for the specialist panel to review the design and make productive recommendations. Even though page 13 of the RFP indicates that Value Engineering will be performed during Final Design, the RFP includes a Contingency Allowance of \$50,000 for Value Engineering, if OMB schedules the workshop during the tail end of Preliminary Design. As such, it is not necessary to include the cost associated with Value Engineering in the proposal. NYCDOT will solicit a cost proposal from the designer utilizing this Contingency Allowance during preliminary design, if required.

Q12. Does a joint venture must have its own EIN number? Does that number have to be established prior to the submission of the Proposal date of August 11th?

A12. A Joint Venture (JV) must have its own EIN number. It does not have to be established prior to submission.

Q13. Kindly confirm that all employees E-4 and above proposed for the project must be registered engineers in New York? Is it acceptable to have the process of reciprocity underway at the time of the proposal, particularly since the execution of any agreement will not take place until July of 2012?

A13. Yes, ASCE Grade IV (A-IV) and above requires a valid New York State Professional Engineer (P.E.) License. Yes, reciprocity is acceptable provided that New York State P.E. approval occurs before the registration of this contract.

Q14. The title of Task # 2 reads: "New Structure Design, Geotechnical Foundation Report, Seismic Analysis/Design, Environmental Assessment Statement, Permits and Design Report". The subsequent subsections cover all the title items with the exception of the "New Structure Design". For example, subsection 2-1 is Geotechnical Foundation Report, 2-2 is Seismic Analysis/ Design and so on. Should there be a subsection for "New Structure Design"?

A14. There is no separate subsection for "New Structure Design." All the subsections of Task #2 and Task #5 pertain to the design of the new structure.

Q15. During the presentation, NYCDOT indicated that the bridge opens at least once a day and that the construction staging should be such that at any time during construction the bridge should be opened for vessels. Is this correct? Is it the intent of the Dept. to not close the bridge at all during construction? If this is not the case how long can the bridge be closed?

A15. Yes, navigational use of this bridge shall be maintained during the construction period. However, the normal drawbridge operation may be temporarily deviated with a US Coast Guard permit.

Q16. On Form 3P – As a new corporate entity, it is unlikely that there will be a corporate seal. Should each JV partner submit its own affirmation?

A16. Yes. However, a Joint Venture corporate seal is required at the time of contract (execution) vendor sign-off.

Q17. Form 1T – attrition rates don't exist within the JV structure, should each JV partner submit its own rates?

A17. Yes, each Joint Venture partner should submit its own attrition rates.

Q18. The JV partners are separate entities. Should the 5T Forms be submitted individually or combined?

A18. The 5T Form should be submitted individually.

Q19. Under Scope of Work: Sheet 2: the second line under "General Scope of Work" states the consultant shall prepare the preliminary design for replacement of the bridge "by advancing/improving the alternative indicated in a copy of the....2010 Feasibility Study..." Is NYCDOT seeking improvements to the alternative?

A19. Yes. NYCDOT will entertain improvements to this alternative as long as it can be demonstrated that it is economical and cost-effective.

Q20. Under Scope of Work: Sheet 3 third paragraph: The second sentence is "The minimum clearances between the proposed bridge fascias and edge of the Bruckner and the Cross Bronx Expressways shall be established during the design process upon approval of the NYC/NYS DOT." Who will establish and/or recommend these minimum clearances? Why are they to be established during the design process? Are there issues with regard to these clearances that have not yet been investigated and/or resolved and, if so, what are the issues?

A20. The intent of the second sentence noted in the question is that actual clearances between the proposed bridge fascias and the edge of the Bruckner and the Cross Bronx Expressways shall be approved by NYCDOT and NYSDOT. There are no issues involved.

Q21. Under Scope of Work: Sheet 3: The first line under "A) SUPERSTRUCTURE" appears to be incomplete; the end is missing. Is there an error?

A21. See revised Page 3R of the Scope of Work.

Q22. Under Scope of Work: Sheet 4: The first bullet at the top of the page is "design new street lighting and traffic signals." With regard to the traffic signals, does this apply to those at the intersections of Bruckner Blvd. with Zerega Ave and with Brush Ave as well as the signals at the bridge? If so, will the work include designing new signal timing?

A22. All work pertaining to street lighting and traffic signals within the project limits specified in the RFP, page 2 of the Scope of Work, shall be evaluated in conjunction with other Divisions within NYCDOT.

Q23. Under Scope of Work: Sheet 4, item E states: "Demolition and construction of the structures shall be performed in stages as indicated below and under section 2.5.c." Please clarify as there appears to be no section 2.5.c.

A23. See revised Page 4R of the Scope of Work.

Q24. Under Scope of Work: Sheet 9 under item "d" it states that the project "may require hydraulic analysis to quantify significant hydraulic effects associated with potential constrictions or obstacles to flow for the structure configuration or construction methods". Will this effort, if required, be included in the "hydrographic survey" contingency item?

A24. No. The \$20,000 contingency allowance is only for the Hydrographic Survey of the channel within the project limits.

Q25. Under Scope of Work: Sheet 11 (Task 4 Feasibility Study for Connections...): Will topographic survey be required to obtain the travel way dimensions and features? The survey

limits defined under item 1.2 on sheet 5 do not include the areas for the Task 4 Feasibility Study.

A25. Topographic survey is not required for this task.

Q26. Is the bridge historic or eligible for inclusion on the state or national register as historic?

A26. Eligibility for inclusion on the state or national register will be determined during preliminary design.

Q27. For Component 3 - Cost Proposal, the instructions state that columns 3, 4 and 5 of Form 4T1 and Form 4T2 are to be completed and submitted in a separate sealed envelope as part of the cost proposal packet. Does that mean there will be an envelope within an envelope? Should there be a version of Form 4T1 with all columns visible accompanying Form 4T3?

A27. Yes. Component 3 should be completed and submitted in a separate seal envelope entitled Component 3 – Cost Proposal Packet. Form 4T1 should have all columns visible accompanied by Form 4T2 and Form 4T3.

Q28. On page 2 of the scope of work under Item A, there's a reference to a PowerPoint of the 2010 Feasibility Study. Is that document available for download?

A28. No. This document is not available for download.

Q29. Please clarify if subcontractors can be specified in our proposal and thus use their participation to meet the DBE goals?

A29. Yes, subcontractors can be specified in your proposal and used to meet the DBE goals. A DBE firm must be certified. A list of DBE firms can be obtained. See Form 6T for the website address.

Q30. Will a new BRPR be required under this contract? The SR pages give that impression. Please verify that both a Design Report and a BRPR is required.

A30. Please refer to the response to Q6.

Q31. Are the design documents to be prepared in English Units? Please confirm this as there are contradictions to this in the SR pages.

A31. The design documents shall be prepared in U.S. Customary Units. Please follow Engineering Bulletin number EB-09-016 issued by NYSDOT dated 4/3/2009.

Q32. Are the load ratings to be done following the Manual of Condition Evaluation using LRFR? The RFP (SR Pages) describes Load Ratings by ASD and LF methods. Please verify that Load Rating is to be done by LRFR Method only.

A32. Load Rating for the new bridge shall be done by LRFR method.

Q33. Is the seismic design to be done by the latest AASHTO, NYSDOT Guidelines? The Bridge is classified as a critical bridge. However the SR Pages state that the design is to be done based on older guidelines. Please confirm that the SR pages provisions do not apply.

A33. The seismic design shall be performed per the latest AASHTO, NYSDOT and NYCDOT Guidelines as stated on page 7 of the Scope of Work.

Q34. In general the SR Pages require that the task be performed as if the available data (In Depth Inspection, BRPR, Geotechnical Report, Hazardous Material Report, etc) has to be redone. The initial pages require that the selected firm verify the information embodied in these documents. Please issue a clarification on this.

A34. As noted during the presentation, a lot of engineering data has been collected, analyzed and documented during the previous contract. The intent of the Department is that the selected firm (Designer of Record) shall first review, validate and utilize as much information as possible from these engineering documents. Additional information, if required, shall then be obtained by the designer for preparation and submission of all required deliverables.

Q35. Can the items listed in the preliminary allowance contingencies page (page 16 of the RFP) be assigned to DBE firms to comply with the DBE requirements?

A35. No. The firm must comply with specified DBE goals from within the actual proposed work.

Q36. Are the reports and documents referenced in the Scope of Work available for review at this time? Particularly the BRPR, the Foundation Report and Minutes of Agency Meetings.

A36. Appointment can be made to review available documents by contacting Mr. Frank Oliveto at 212-839-6317.

Q37. Are tables considered graphics and therefore prohibited from use in the response?

A37. No. Tables are permitted in your submission.

Q38. What level of coordination has been done with NY State so far? Are any state approvals required?

A38. This is a federally funded project, with funds administered through NYSDOT and, therefore, approval from the State will be required at various design stages. It is strongly recommended that proposers familiarize themselves with the NYSDOT issued manual for Procedures for Locally Administered Federal Aid Projects.

Q39. Component 4, Local Law 34, is this to be submitted by all team members or just the Prime Consultant?

A39. Component 4, Local Law 34 should be submitted by the Prime Consultant.

Q40. Please confirm only the forms specifically listed in the checklist need to be submitted with our proposal - the forms listed at the back of the RFP are not required at proposal time.

A40. Yes, the forms listed on the B. Proposal Package Contents (“Checklist”) are to be submitted along with Disclosure of Lobbying Activities, Certification of a Contractor Regarding Debarment, Suspension and other Responsibility Matters, Certification of a Subcontractor/Supplier Regarding Debarment, Suspension and other Responsibility Matters and DBE Participation Goals for FTA projects.

Q41. Will the project require EIS?

A41. The intent of the Department is to prepare a Design Approval Document in accordance with NYSDOT’s Project Development Manual, which includes an Environmental Assessment. See Section 2-3 on page #8 of the RFP.

Q42. Will there be a site visit made available, if so please inform us of a date and time?

A42. A site visit will be on Wednesday, August 3, 2011 at 10am at the bridge site. We will meet in front of the Operator House. Each firm will be allowed to have maximum of two people. Please note that Government issued ID is required for the site visit. Hard Hat and Vest is mandatory.

*** All firms must email Frank Oliveto (foliveto@dot.nyc.gov) and Dr. Paul-Michael Kazas (pkazas@dot.nyc.gov) to confirm their availability and there proposed two (2) representatives prior to Wednesday August 3, 2011.**

Michael R. Bloomberg
Mayor

The City of New York
Department of Transportation
Division of Bridges

Janette Sadik-Khan
Commissioner

REQUEST FOR PROPOSALS

FOR

**TOTAL DESIGN AND CONSTRUCTION SUPPORT SERVICES FOR
REPLACEMENT OF BRUCKNER EXPRESSWAY OVER WESTCHESTER CREEK
(UNIONPORT BRIDGE)
BOROUGH OF THE BRONX**

CONTRACT NO.: HBX1131

E-PIN: 84111M0005

PIN: 84111BXBR587

RELEASE DATE OF THE RFP: July 14, 2011

**ANTICIPATED CONTRACT TERM: 3073 Consecutive Calendar Days from the Date of
Written Notice to Proceed.**

AUTHORIZED AGENCY CONTACT PERSON

Proposers are advised that the Authorized Agency Contact Person for all matters concerning this Request for Proposal is:

Dr. Paul-Michael Kazas
Director, Capital Procurement
55 Water Street, 5th Floor
New York, NY 10041
Telephone: (212) 839-6314
Fax: (212) 839-4043

SECTION III - SCOPE OF SERVICES

A. Agency Goals and Objectives

The agency's goals and objectives are to acquire quality Total Design and Construction Support Services to ensure the safety of New York City bridges in relation to their structural integrity and repair as stipulated in Section VII of the RFP.

B. Agency Assumptions Regarding Consultant Approach

The agency's assumptions regarding which approach will best achieve the goals and objectives set out above are reflected in the Proposed Contractual Agreement. (See Section VII, Attachment A)

C. Proposed Contractual Agreement

Each successful proposer shall negotiate an agreement based on the Proposed Contractual Agreement (See Section VII, Attachment A). Such agreement shall contain the appropriate United States Department of Transportation and New York State Department of Transportation requirements, including but not limited to "NYSDOT procedures for Locally Administered Federal Aid Projects"

In addition, the proposers must submit the "Disclosure of Lobbying Activity" and "Certification of Contractor regarding Debarment, Suspension and other Responsibility Matters" attached in Appendix A-II with its Technical Proposal.

D. Opportunity for Disadvantaged Business Enterprise

The NYC Department of Transportation encourages firms to meet New York State's Disadvantaged Business Enterprise (DBE) utilization goal of **18%**. The successful proposers must show good faith efforts that it attempted to meet the DBE goal.

- ◆ List of certified DBE firms can be obtained from the following website:
<http://biznet.nysucp.net/>

E. Compliance with Local Law 34 of 2007 (See Attachment 'G')

Pursuant to Local Law 34 of 2007, amending the City's Campaign Finance Law, the City is required to establish a computerized database containing the names of any "person" that has "business dealings with the City" as such terms are defined in the Local Law. In order for the City to obtain necessary information to establish the required database, **vendors responding to this solicitation should complete the Doing Business Data Form (see Attachment H) and return it with this proposal.** The submission of a Doing Business Data Form that is not accurate and complete may result in appropriate sanctions.

The Unionport Bridge shall be designed to meet the latest requirements of the AASHTO Load and Resistance Factor Design (LRFD) Specifications for Movable Bridges and the AASHTO LRFD Specifications for Highway Bridges, the "LRFD Blue Pages", the Guide Manual for Condition Evaluation and Load and Resistance Factor Rating (LRFR) of Highway Bridges, the NYS Standard Specifications for Highway Bridges, and latest NYS Engineering Instructions and Bulletins. The Consultant shall consult the NYSDOT Bridge Manual (BM) and the NYSDOT Highway Design Manual (HDM) for policies, guidance, details and interpretation of the design specifications.

Roadway design shall follow the latest AASHTO standards for geometric controls and Work Zone Traffic Control (WZTC) plans criteria and the latest versions of the Manual on Uniform Traffic Control Devices (MUTCD).

Navigation channel clearances shall meet or exceed the existing clearances. The minimum clearances between the proposed bridge fascias and edge of the Bruckner and the Cross Bronx Expressways shall be established during design process upon approval of the NYC/NYSDOT. In regard to the Life Cycle Cost considerations, the Estimated Service Life shall be 75 years. The Preliminary Plans shall be prepared in English units.

The general scope of work shall include, but not limited to, the following major items:

A) SUPERSTRUCTURE:

- Demolish the existing Bascule, Flanking and Approach spans in stages (see MPT under item E).
- Design new superstructure for the Bascule, Flanking and Approach/Ramp spans in stages. The existing double leaf, fixed trunnion, bascule bridge will be replaced with a new movable span of bascule type.

B) SUBSTRUCTURE:

- Demolish, as required, the existing piers/abutments of the Bascule, Flanking and Approach/Ramp spans in stages
- Design new substructures for the Bascule, Flanking and Approach/Ramp spans in stages.

C) APPROACHES:

- Demolish the existing approach slabs/ramps and roadways in stages as required.
- Design new approach slabs, ramps and roadway pavements in stages as required within the project limit including intersections at Zerega Ave/Ramps and at Brush Ave/Bruckner Boulevard.

D) UTILITIES:

- Replace and maintain the existing utilities as required within the project limits.
- Protect, support and maintain the existing NYCDEP sanitary/sewer utilities during construction. The Consultant shall receive a written approval from NYCDEP and comply with their comments and requests.
- Provide coordination with private utilities, including Con Edison for electrical conduits and vault under the approach structures.
- Design new submarine cables under the Westchester Creek
- Remove all existing utilities in the Control and Operator houses and design new utilities in the new Control/Operator house.
- Design new scuppers and drainage system.

- Design new street lighting and traffic signals.
- E) WORK ZONE TRAFFIC CONTROL (WZTC):
Demolition and construction of the structures shall be performed in stages as indicated below under section 2-4(f).
- F) OPERATOR/CONTROL HOUSES:
- Demolish the existing Operator and Control Houses including machinery and all utilities.
 - Design new Operator and Control Houses including water, sewer, HVAC, electrical, and telephone utilities, fire alarm, lighting, power, etc., and mechanical system.
 - Design Temporary Operator House and temporary facilities as required.
- G) ELECTRICAL and MECHANICAL:
The Engineer shall determine and show the following on the contract plans as a minimum:
- Removal of the existing electrical and mechanical systems of the bascule bridge
 - New electrical and mechanical systems of the new movable bascule bridge
 - New electrical equipment
 - New Programmable Logic Controller (PLC)
 - New Motor Drive System
 - New Fire Alarm system
 - New Security System
 - New Emergency Drive System
 - New Pigeon Proofing (all machinery areas)
 - Balancing of bascule span during and after construction
 - Temporary power distribution
 - Temporary control system
 - Phasing sequence
- H) TRAFFIC CONTROL SYSTEM:
- Design new Warning Gates and Barrier gates
 - Design new Traffic Signals
 - Design new Sign structure(s)
 - New stripping and pavement marking
- I) FENDER SYSTEM:
- Remove and replace the existing fender system, sheeting and dolphins with new system.

All of the above items of work shall be accomplished under one or more of the following **specific major tasks** and services at a minimum:

SPECIFIC SCOPE OF WORK:

Task 1: Field Investigations

1.1 Collection of Information and Record Retrieval:

- a) The Engineer shall obtain and review all available information and record data. To the extent feasible, the Bridge Reconstruction Project Report (BRPR) of 1998 may be utilized to develop the Design

FORM 4T1-R – LABOR COST PROPOSAL

PROJECT NAME: Total Design & Construction Support Services for the Replacement of Bruckner Expressway over Westchester Creek (Unionport Bridge)

E-PIN: 84111M0005
PIN: 84111BXBR587

BIN: 1-06651-0

PRIME CONSULTANT: _____

CONTRACT NO.: HBX1131

CONSULTANT ON THIS FORM: _____

	<input type="checkbox"/>	PROFESSIONAL ENGINEERING/ARCHITECTURAL SERVICES			
	<input type="checkbox"/>	OTHER/ _____			
(COLUMN 1) JOB TITLE ASCE/ NICET GRADE	(COLUMN 2) TOTAL HOURS	(COLUMN 3) HOURS THIS FIRM	(COLUMN 4) AVERAGE HOURLY RATE (FY 2012)	(COLUMN 5) LABOR COST COL3X COL4	
1.	_____	_____	\$ _____	\$ _____	
2.	_____	_____	\$ _____	\$ _____	
3.	_____	_____	\$ _____	\$ _____	
4.	_____	_____	\$ _____	\$ _____	
5.	_____	_____	\$ _____	\$ _____	
6.	_____	_____	\$ _____	\$ _____	
7.	_____	_____	\$ _____	\$ _____	
8.	_____	_____	\$ _____	\$ _____	
9.	_____	_____	\$ _____	\$ _____	
TOTALS	_____	_____		\$ _____	(T)

INTERIM OVERHEAD FACTOR _____ (A)

PROFIT FACTOR _____ (B)

INTERIM MULTIPLIER _____ (1+A)X(1+B) = M

TOTAL LABOR COST (T x M) \$ _____ (C)

MAXIMUM ESCALATION FACTOR = **1.06(D)**

TOTAL LABOR ESCALATED
TO PROJECT MIDPOINT
(GRAND TOTAL COST) (C X D) \$ _____ (E)

INSTRUCTIONS:

1. Each consultant of the project team is to submit a separate "Labor Cost Proposal Form". For each job title, the hours proposed by each firm of the project team in Column (3) **MUST SUM** to the total hours provided in Column (2).
2. For Column (4), use actual average salary rates for firm for each job title at regional offices. Attach a listing of current average rates for all titles/grades/levels as approved by NYSDOT for regional offices (if available). A regional office is defined as one located within a 75 mile radius of Columbus Circle (NYC).
3. The labor costs to be included in Column (5) are obtained by multiplying the hours in Column (3) by the average hourly rate in Column (4).
4. The maximum escalation factor "D" indicated in the shaded area shall not be changed.
5. Interim Multiplier (M) shall be rounded off to three (3) decimal figures. Total Labor Cost (C) and Column 4 & Column 5 entries shall be rounded off to two (2) decimal places.
6. The agency will consider the proposed interim multiplier for establishing Preliminary Design Total Fee (including DTL, interim overhead & Maximum Profit of 10%). The interim multiplier will be based on currently available information on Consultant Company's overhead and profit. This multiplier is subject to audit and revision in accordance with applicable NYC Comptrollers' Directive on an annual basis when the actual overhead information for the respective year becomes available. Suitable adjustments to the previous payments will be made accordingly upon completion of phase and when multiplier information is available. As needed, the additional fund for the overhead and profit will be added via Change Order by the Agency at the discretion of the Agency. The same procedure will be followed at the beginning of Final Design & CSS Phases.

FORM 4T2-R – COST PROPOSAL SUMMARY

PROJECT NAME: **Total Design & Construction Support Services for the Replacement of
Bruckner Expressway over Westchester Creek (Unionport Bridge)**

E-PIN: **84111M0005**
PIN: **84111BXBR587**

BIN: **1-06651-0**

PRIME CONSULTANT: _____ CONTRACT NO.: **HBM1165**

	<u>(COLUMN 1) CONSULTANT</u>	<u>(COLUMN 2) HOURS ALL FIRMS</u>	<u>(COLUMN 3) ESCALATED LABOR COST TO PROJECT MIDPOINT</u>	<u>(COLUMN 4) DIRECT NON-SALARY COST</u>	<u>(COLUMN 5) TOTAL COST</u>
1.	_____	_____	_____	\$ _____	\$ _____
2.	_____	_____	_____	\$ _____	\$ _____
3.	_____	_____	_____	\$ _____	\$ _____
4.	_____	_____	_____	\$ _____	\$ _____
5.	_____	_____	_____	\$ _____	\$ _____
6.	_____	_____	_____	\$ _____	\$ _____
7.	_____	_____	_____	\$ _____	\$ _____
8.	_____	_____	_____	\$ _____	\$ _____
9.	_____	_____	_____	\$ _____	\$ _____
	TOTALS	_____	_____	\$150,000.00	(T)
	Budgetary Allowance	_____	_____		\$15,000.00
	Contingency Allowance	_____	_____		\$495,000.00
	GRAND TOTAL	_____	_____	_____	_____

INSTRUCTIONS:

1. The costs entered in Column 3 are the totals shown on line (E) of Form 4T1-R "Labor Cost Proposal" for each consultant on the project team.
2. The Total Direct Non-Salary Cost shown in the shaded area below Column 4 is an out of pocket expense budgeted amount allowed to all proposers and must not be changed.
3. The Total Direct Non-Salary Cost provided by each consultant of the project team MUST SUM to the total shown in the shaded area at the bottom of the Column 4.

**NYCDOT Division of Bridges
Movable Bridge Group**

**Bruckner Expressway
over
Westchester Creek
(Unionport Bridge)**

**Borough of the Bronx
Contract No. HBX1131**

July 20, 2011

Unionport Bridge



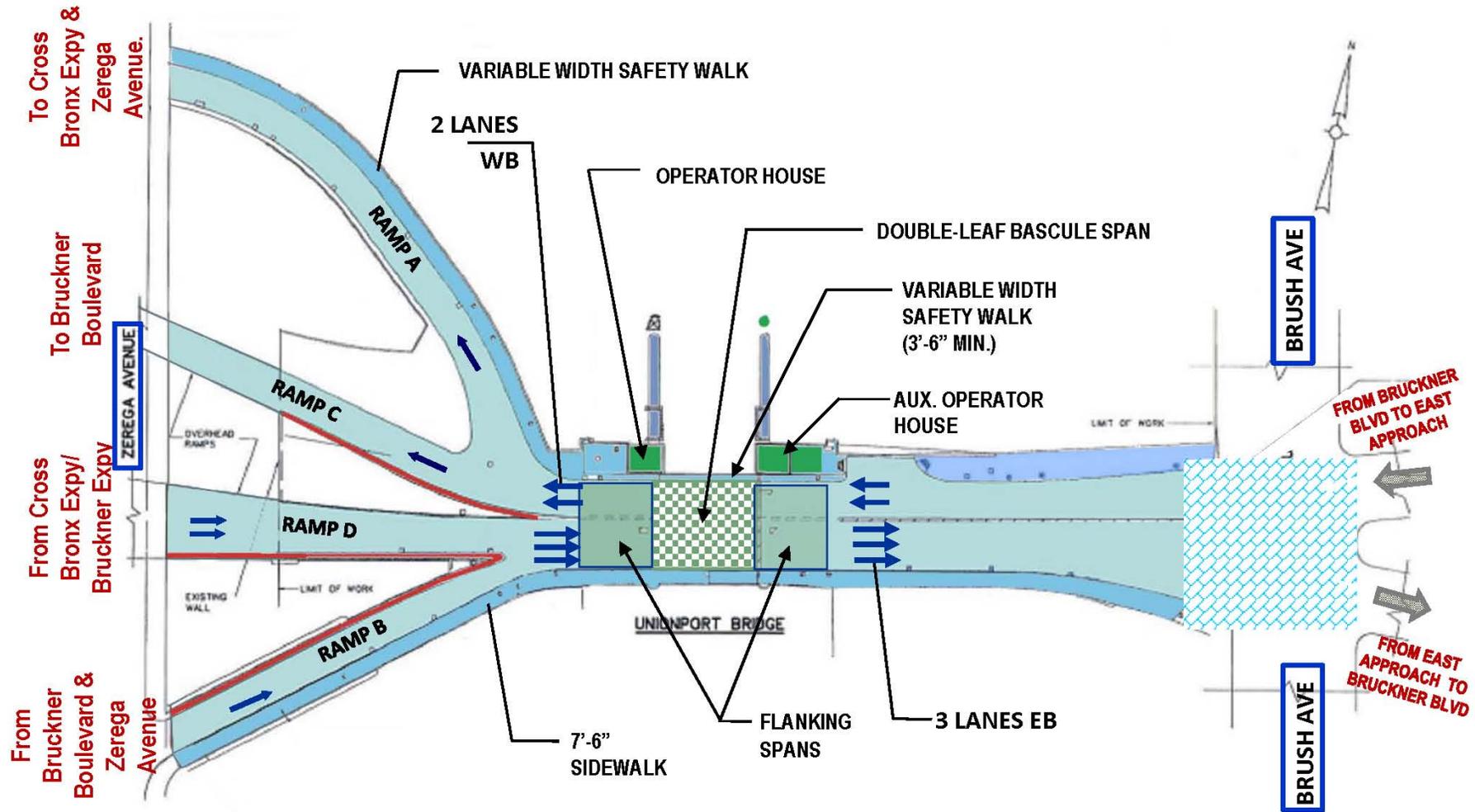
- Provides freeway connections between:
 - Bruckner Expressway
 - Cross Bronx Expressway
 - Hutchinson River Parkway
- Provides local connection across Westchester Creek between Unionport and Throgs Neck neighborhoods
- Movable span opens approximately once per day to allow vessels to access oil storage facility and marina upstream of the bridge

NEW YORK CITY



NYCDOT
Division of Bridges

Existing Bridge General Plan

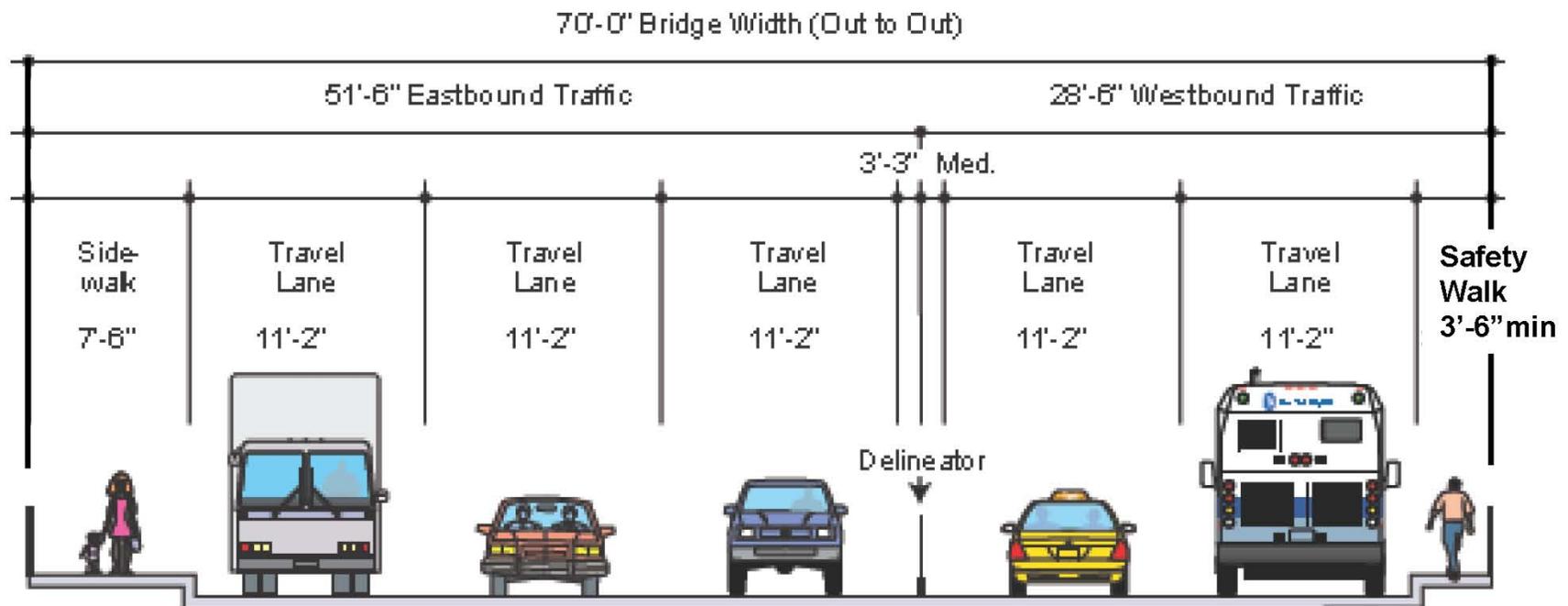


NEW YORK CITY



NYCDOT
Division of Bridges

Existing Bridge Section

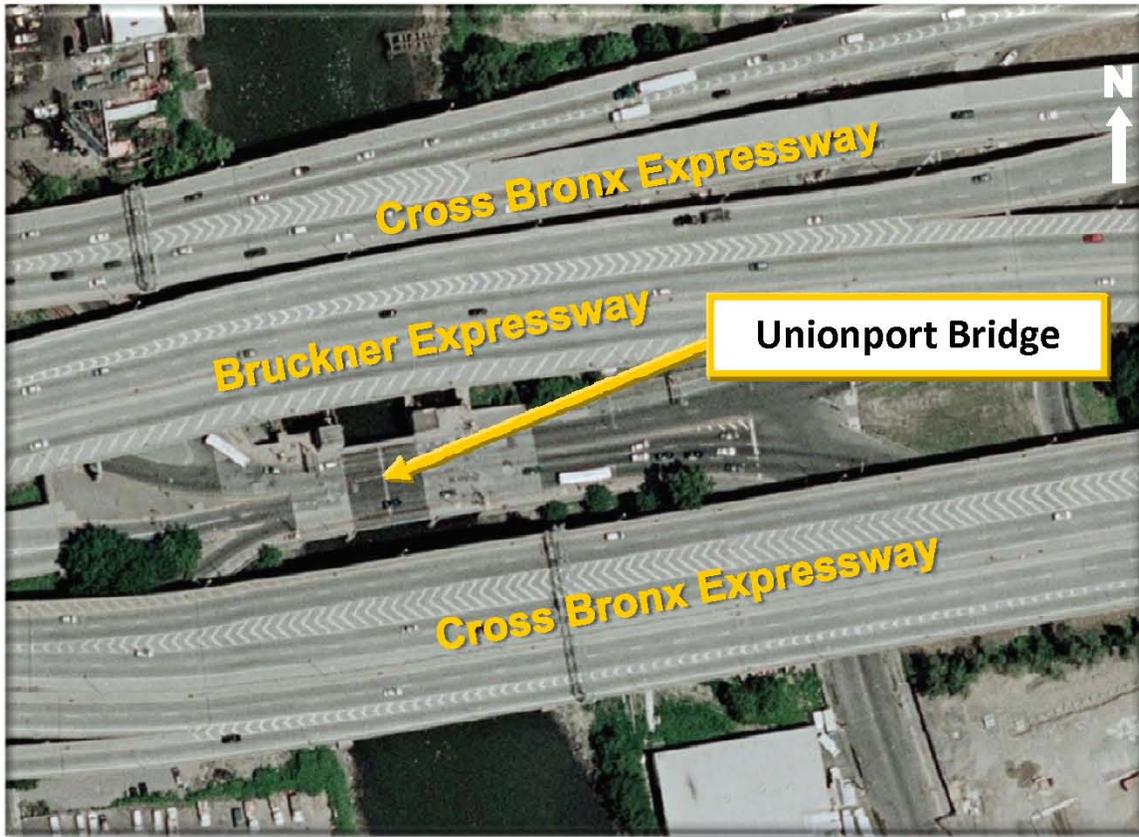


**Existing Bridge Section
(looking west)**

EXISTING UTILITIES:

- Con Edison conduits and vaults, located under the bridge approaches
- Submarine cables under the Westchester Creek
- NYCDEP utilities under the approach structures
- Fire alarm and telephone conduits under the north sidewalk of Ramp A
- Drainage scuppers
- Street lighting and traffic signals

Unionport Bridge Traffic



Traffic Data (2008)

- 65,000 ADT
- 2,200 Bi-Directional Peak Hour Volumes

NEW YORK CITY



NYCDOT
Division of Bridges

EXISTING BRIDGE (Eastbound View)



Existing Bridge (Westbound View)



NEW YORK CITY



NYCDOT
Division of Bridges

NYC Cycling Map



- Bicycle Path, Class 1 (bridge, park or separated on-street path) █
- Bicycle Lane, Class 2 (two-way unless shown) (on-street striped route) █
- Bicycle Route, Class 3 (on-street signed route) █
- Planned/Proposed Bicycle Path (for future construction) █
- Planned/Proposed Route ●

MAINTENANCE OF TRAFFIC REQUIREMENTS

Construct new bridge in Construction phases:

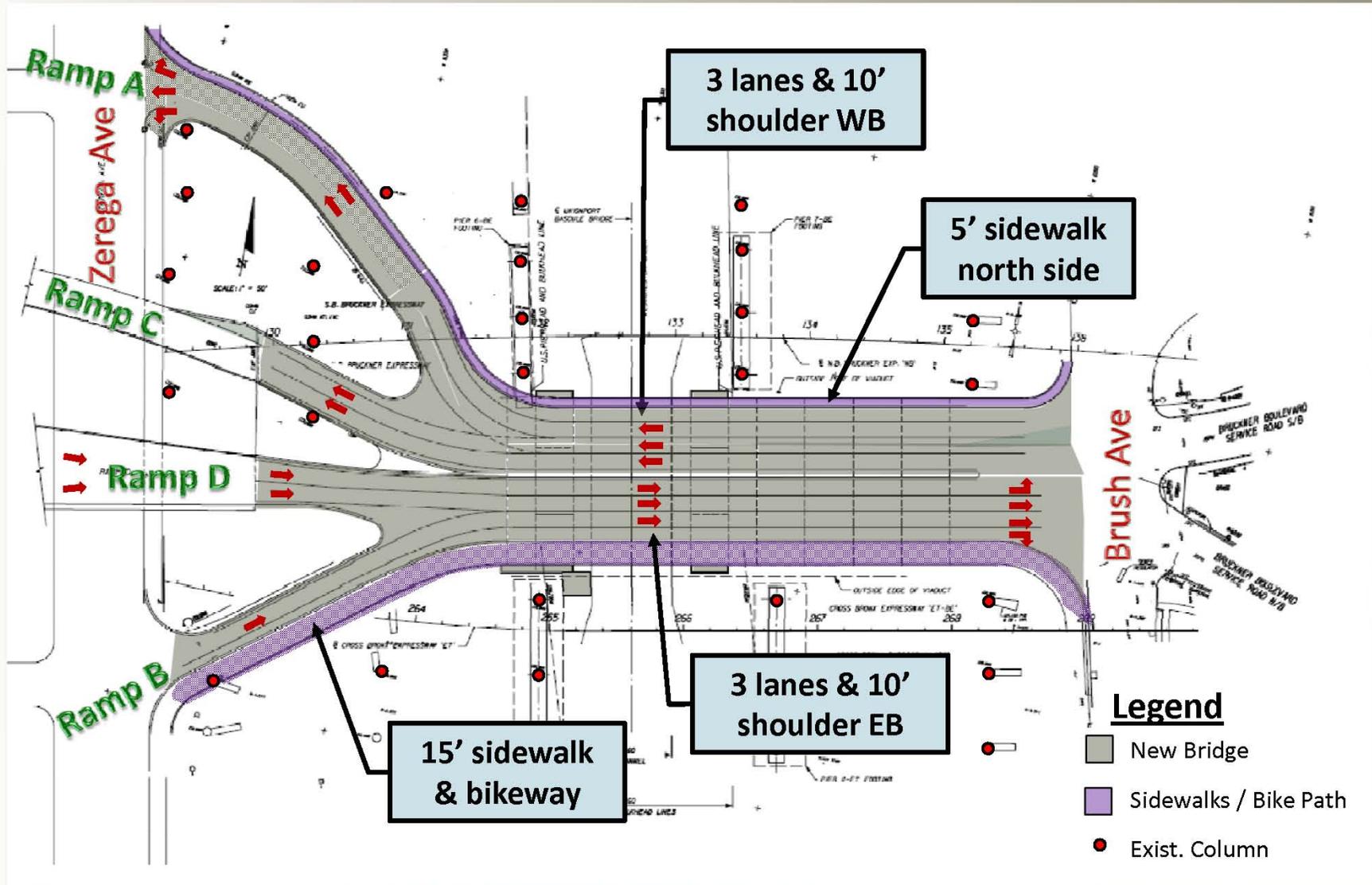
- **Maintain two lanes of traffic in each direction**
- **Maintain all ramp movements (minimum of one lane)**
- **Maintain at least one sidewalk**
- **Maintain navigation channel**

AVAILABLE DOCUMENTS:

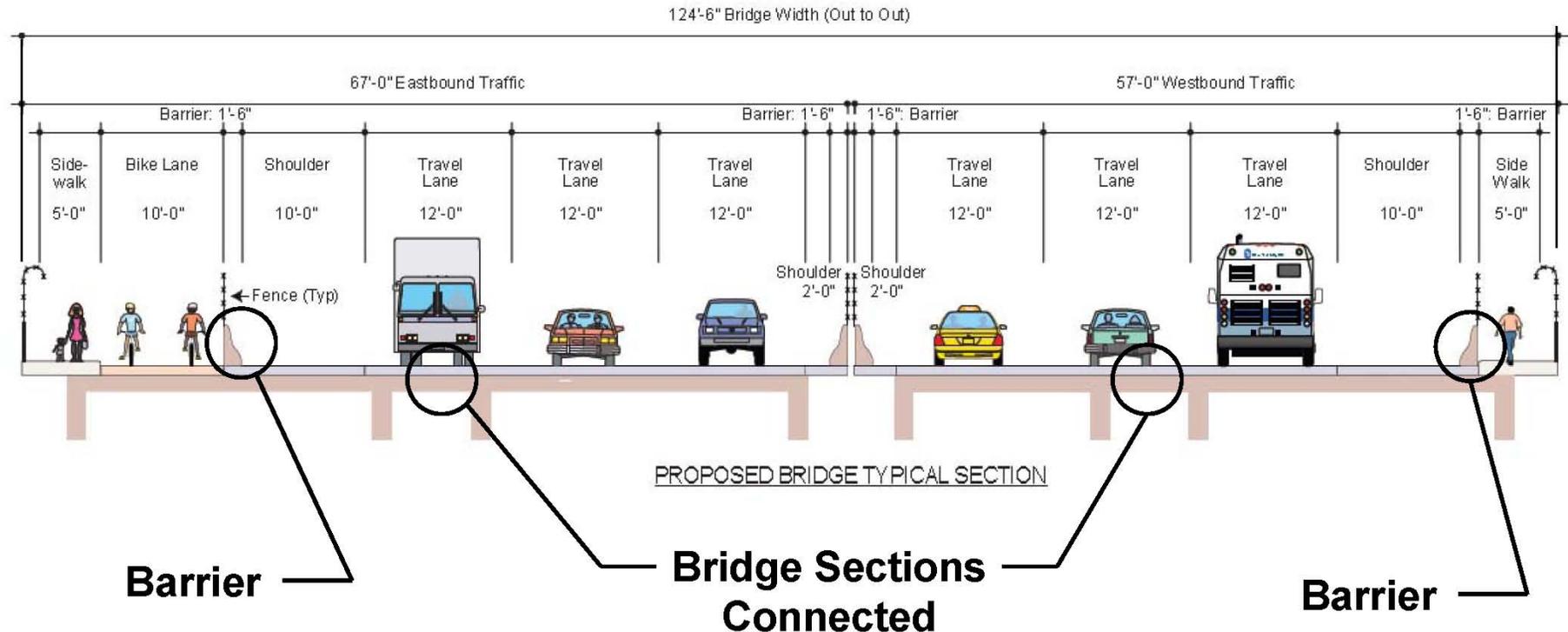
- BRPR 1998
- Geotechnical Foundation Reports 1999 & 2004
- Hazardous Material Assessment Report 1998
- Phase II Environmental Site Investigation Finding Report 2004
- Traffic Count Data Reports 1999 & 2007
- Inspection Reports various
- Feasibility Study for Replacement of the Bridge 2010

These available documents shall be validated and used as much as possible by advancing/improving the replacement alternative into the design of the Unionport Bridge.

Bridge Replacement - Final Configuration

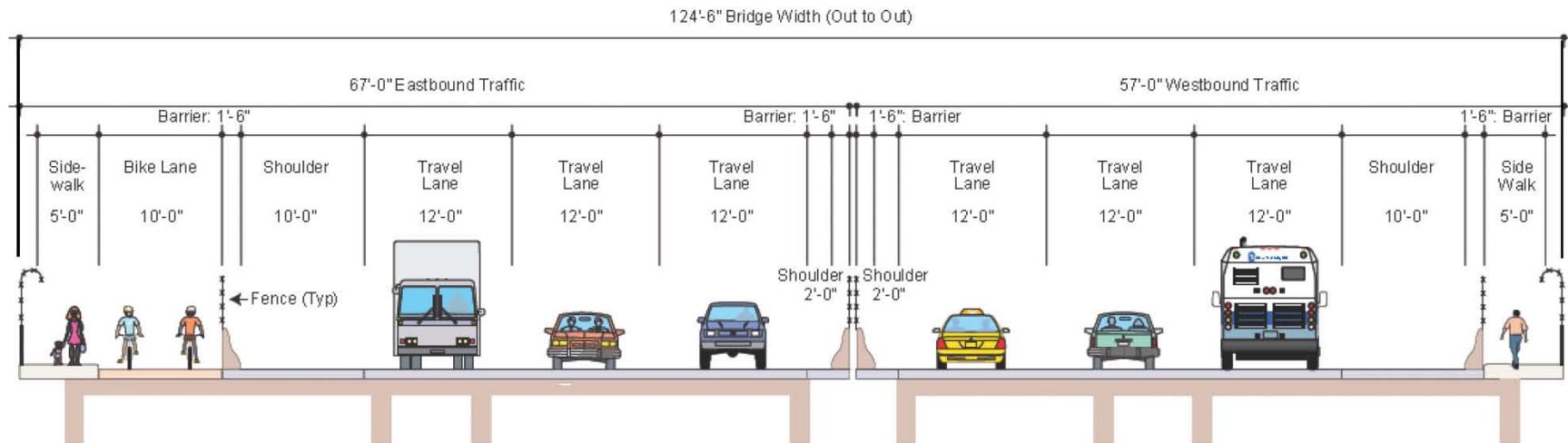


Replacement Bridge Section

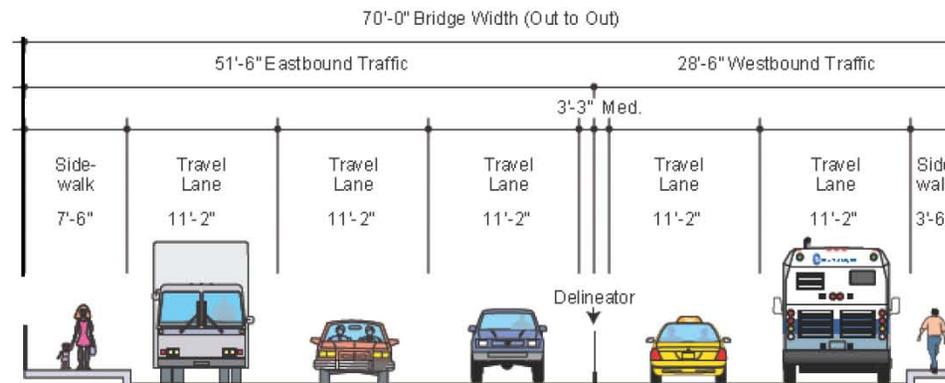


The final section is a twin, double-leaf bascule, with operational redundancy.

New vs. Existing Bridge Sections



Proposed Typical Section



Existing or Rehabilitated Typical Section

PROJECT REQUIREMENTS:

- The new bridge will be a movable bridge
- This project will be Federally-funded and, therefore, the Design Report must be prepared.
- Design shall be comply with current federal, state and city standards and requirements, using LRFD method
- Design shall be prepared in English Units, as per the scope of services
- Navigation channel clearances shall meet or exceed the existing
- Value Engineering Study
- Constructability review
- Incentive/Disincentive Clauses
- ULURP
- The construction schedule shall be optimized to minimize inconvenience to the community and traveling public

**Total Design and Construction Support Services for
Replacement of Bruckner Expressway
over Westchester Creek (Unionport Bridge),
Borough of the Bronx**

Contract No. HBX1131
E-Pin No. 84111M0005
Pin No. 84111BXBR587

**Pre-Proposal Conference
SIGN - IN SHEET**

Wednesday, July 20, 2011 @ 2:00pm

55 Water Street, Bid Room

PLEASE PRINT NAME:	REPRESENTING:	TELEPHONE #:
GEORGE KORLEY	STV INC	212-614-3306
DONALD YUE	STV INC	212-614-3312
JASON VESUVIO	Ellana (WBE)	212-971-0936
JOHN BALISON	HVERA	945 838-3600
Kishanth Balachandran	Ewell W. Finley, P.C	212-695-7047
Ayman Baki	Tectonic	718-640 1000
MUHAMMAD SIDDIQU	SIMCO (MBE)	212-385-8100
WARREN MICHELSON	SIMCO ENGINEERING	212-385-9100
Eric Kelly	HNTB	212 594 9717
Robert LaMagna	Jacobs	212-944-2000 x6117
Kim Murray	" "	973-267-8830 x1250
S. DAVID ALI	Infra Tech Engrg.	212-514-7500
DAVID NYARICO	P B	212-465-5534
G Fanyang	Weidlinger	212 367 3042
JOE DUCKEY	CHEMTECH (MBE)	732-688-2642
HASEEB AHSON	TechniQuest Corp	732 274 9500
Linda Beardon	The RIBA Group	212 741 8090 x-153
JENNIFER M. CATO	"	212-741-8090 x 7149

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PLEASE PRINT NAME:	REPRESENTING:	TELEPHONE #:
MICHAEL ZOENWIK	HDR	973-474-5014
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Michael Hershey	En Tech	646-722-0000
Sue Bayat	"	"
Muhammad Iqbal	SI Engineering	212-354-5938
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MIKE ABRAHAMIS	PARSONS BRINCKERHOFF	212-465-5185
Sara Speicher	Sam Schwartz Engineering	212 598 9010 x135
KEN MANGAM	HAKS	212-747-1997 x726
Marc Lewis	Geac Corp. (Land Survey)	908-797-6682
XXXXXXXXXX		
JEFF. REYT	PARSONS	212 266 8487

THE CITY OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF BRIDGES

ACKNOWLEDGEMENT RECEIPT OF ADDENDUM #1

Total Design and Construction Support Services for
Replacement of Bruckner Expressway
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Borough of the Bronx
Contract No. HBX1131
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Pin No. 84111BXBR587

Addendum #1

TOGETHER WITH ALL WORK INCIDENTAL THERETO

I, _____
(NAME AND TITLE)

A duly authorized representative of
(NAME OF PROPOSERS)

Acknowledge receipt of Addendum No. 1 dated
July 29, 2011 for the Contract No. HBX1131